



8.13 HAMMOND DEVELOPMENT PERMIT GUIDELINES

7279-2016



Hammond Area Plan

Development Permit Area Guidelines

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Contents

Overview and Purpose	124
Hammond Development Permit Area.....	124
Justification	125
Organization of the Guidelines	125
 Part 1 Introduction	 127
Hammond Neighbourhood Context.....	130
Hammond Heritage and History	130
 Part 2.....	 133
Neighbourhood Precincts	133
Hammond Precincts	135
Precinct Description & Key Guideline Concepts for Development	136
 Part 3.....	 145
Development Permit Area Guidelines	144
Application	146
1.0 General Guidelines	147
2.0 Multi-family Residential (Low and Medium Density).....	148
3.0 Hammond Village Commercial.....	154
4.0 Infill General Employment.....	160

List of Figures

Figure 1. Map showing concentration of heritage sites).	129
Figure 2. Images of Hammonds variety of existing uses, character, look and feel.	134
Figure 3. Hammond neighbourhood precincts.	135
Figure 4. North Hammond precinct (grey).	136
Figure 5. Images of typical development in North Hammond.	137
Figure 6. Upper Hammond precinct (grey).	138
Figure 7. Images of typical Upper Hammond precinct homes.	138
Figure 8. Landscaping in Upper Hammond	139
Figure 9. Townhouse development.	139
Figure 10. Lower Hammond precinct (grey).	140
Figure 11. Images of typical Lower Hammond precinct homes.	140
Figure 12. Maple Meadows Business Park precinct (grey).	142
Figure 13. Images of the Maple Meadows Business Park.	142
Figure 14. Landscaping in Maple Meadows Business Park	143
Figure 15. Maximize opportunities for solar gain and solar collection.	147
Figure 16: Maintain the typical pattern of access.	148
Figure 17: Siting Development Permit Guidelines	148
Figure 18: Example of a duplex that reduces the prominence of front vehicle access.	149
Figure 19: Tri-plex with heritage character and front access with parking at the side.	149
Figure 20. Articulated building face breaks up the massing.	149
Figure 21: Guideline 1.1.9 A transition of scale and density for adjacent land uses.	149
Figure 22. 4 storey apartment building steps down to transition to adjacent existing two storey.	150
Figure 23. Childrens playspace within a multi-family development.	150
Figure 24: Landscaping in parking areas.	150
Figure 25: Siting Development Permit Guidelines	151
Figure 26: Provide direct pedestrian pathways within new developments.	151
Figure 27. Landscape buffers provide screening to parking and utility areas.	151
Figure 28. Semi-private seating/waiting area along the street next to multi-family development.	152
Figure 29. Multi-family development with verandah, overhang, and use of horizontal wood siding.	152

Figure 30: New multi-family development shall borrow existing styles and materials.	152
Figure 31: Use of wood siding, columns, and multi-paned windows.	153
Figure 32: Direct internal pedestrian access..	153
Figure 33. Shops set at property line and sidewalk, facing the street.	154
Figure 34: Mixed use with residential units above and smaller retail shops below.	154
Figure 35: Mixed use development, articulated horizontal massing.	154
Figure 36: Landscape screening and side parking lot.	155
Figure 37. Provide for outdoor patio space and allow activity to spill out onto the street.	155
Figure 38. Seating areas, paving patterns and planters.	155
Figure 39. Incorporate public art that references heritage, adds vibrancy, entertains or educates.	156
Figure 40. Use multi-paned windows to create a historic look and feel.	156
Figure 41. Incorporate patterns and materials that reflect aspects of Hammonds heritage.	156
Figure 42. Commercial open to the street, incorporating patio space.	157
Figure 43: Recessed, clear entry with weather protection awning and signage above.	157
Figure 44: Smaller retail spaces, multiple entries, transparent front and visible signage.	157
Figure 45. Temporary sandwich board signs.	158
Figure 46: Small recessed courtyard shops with blade signage and potted planting at entrance.	158
Figure 47: Gooseneck lighting highlights signage and directs light.	159
Figure 48: Development addresses the street and blends well with surrounding residential area.	160
Figure 49: Pedestrian and bicycle amenities are strongly encouraged.	160
Figure 50: A friendly face to the street	161
Figure 51: Parking located to the side or rear of the building.....	161
Figure 52: General employment guidelines.	162

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Overview and Purpose

Hammond Development Permit Area

The Hammond Development Permit Area (DPA) is designated pursuant to Section 488(1) of the *Local Government Act*, specifically the following sub-sections:

- (a) protection of the natural environment, its ecosystems and biological diversity;
- (b) protection of development from hazardous conditions;
- (d) revitalization of an area in which a commercial use is permitted;
- (e) establishment of objectives for the forms and character of intensive residential development;
- (f) establishment of objectives for the form and character of intensive residential development;
- (h) establishment of objectives to promote energy conservation;
- (i) establishment of objectives to promote water conservation;
- (j) establishment of objectives to promote the reduction of greenhouse gas emissions.

for all properties within the Hammond Area Plan as identified on Schedule 1 of the Official Community Plan, Bylaw No. 7060-2014.

The Hammond DPA Guidelines apply to the following Hammond Area Plan land use designations and development of:

- Low Density Multi-family
- Medium Density Multi-family
- Infill General Employment
- Hammond Village Commercial

The Hammond DPA Guidelines outline design criteria for new development. These Development Permit Guidelines work in tandem with policies in the Hammond Area Plan and regulations in the City of Maple Ridge Zoning Bylaw, which must also be taken into consideration for Development Permit approval. Other accompanying documents (bylaws, codes) and resources may need to be consulted during the development proposal process.

In the event of a conflict between the Hammond DPA Guidelines and the Hammond Area Plan Land-Use Designations on “Schedule 1” adopted by the City, the latter take precedence. In the event of a conflict between the Hammond DPA Guidelines and regulations outlined in the City of Maple Ridge Zoning Bylaw, the latter two should take precedence. However, in the event of a conflict between Hammond DPA Guidelines and other Maple Ridge DPA Guidelines, the Hammond DPA Guidelines take precedence.

Justification

The purpose of the Hammond DPA Guidelines is to:

- maintain the unique neighbourhood character identified within each precinct, particularly key heritage elements identified through the neighbourhood area planning process and public engagement work and outlined in the area plan guiding principles.
- support high quality design, revitalize the Hammond commercial node, improve connectivity and pedestrian safety in the neighbourhood,
- enhance housing affordability through encouraging energy and water efficiency,
- mitigate for flooding through retention of greenspace and mature trees,
- help the City achieve greenhouse gas emission targets embedded in the OCP.

Organization of the Guidelines

The guidelines document is divided into three main parts:

Part One provides the context and background for the Hammond DPA Guidelines and identifies heritage sites that contribute to the character of the neighbourhood.

Part Two outlines each Hammond Neighbourhood Precinct and defines the characteristics and design elements and the intent for new development.

Part Three contains the development permit guidelines based on Parts 1 and 2. The guidelines outline the desired character, form and design elements to maintain and enhance the character, look and feel of the Hammond neighbourhood.

Concept sketches, diagrams and images are provided with the guidelines as reference only to illustrate how the objectives of the development guidelines might be achieved through design.



Hammond Neighbourhood

Hammond is a unique and important neighbourhood in Maple Ridge with distinct character and great potential for future growth. However, with growth and change, it is important the neighbourhood retain its current charm and characteristics that make it the special place it is today and to ensure smart and sensitive development that respects and retains current and encourages future residents.

Part 1 and 2 of the Hammond Development Permit Area Guidelines describe the context, history and important character aspects of the neighbourhood as the context for Part 3: Guidelines. For additional and more detailed information, explanation of the planning process or overview of community input that outlined these character elements, see the Hammond Area Plan.



Plan of the Town of Port Hammond Junction, 1882

Part 1 Introduction



Hammond Neighbourhood Context

The Hammond neighbourhood is located in the south west corner of Maple Ridge. The area is south of Lougheed Highway and directly adjacent to the Fraser River. The community is divided by the rail line which runs north/west and separates the neighbourhood from the Fraser River on the east side, splitting the neighbourhood in half. This is an active commercial rail line which also supports a nearby West Coast Express Commuter Station at the northwest corner of the neighbourhood. The area is primarily residential, but contains an historic commercial centre and significant employment lands in the Maple Meadows business park and Mill site. Primary access into and out of the neighbourhood is via Lougheed Highway and Golden Ears Way.

While many older aspects of the neighbourhood remain intact, the area has seen recent interest in redevelopment due to a number of strong community assets, including:

- good transportation access;
- a nearby rail commuter station;
- riverfront property potential;
- employment lands;
- small commercial node; and
- a unique historic quality with heritage features remaining.

With continued growth pressure throughout the Lower Mainland, Hammond will redevelop and change over time. As such, there is opportunity to build on the neighbourhood's unique and historic qualities valued by the community.

Hammond Heritage and History

At the heart of the Hammond community is the historic townsite and commercial node. Surrounded by a residential area, two central parks, and oriented to the Mill site and activities on the Fraser River, the node forms a unique enclave within the larger neighbourhood, containing significant history and historic value. With this in mind, a heritage character study identified key heritage features and sites within the original 1882 Port Hammond Junction Township plan boundaries.

The historic places with contributing heritage character and value are identified as follows:

Heritage Revitalization Agreement - Legally Protected Heritage Sites. Statements of Significance have been prepared for the two buildings on this site.

Heritage Register - Recognized for their heritage value, and officially listed on the Maple Ridge Community Heritage Register. Statements of Significance have been prepared for these two sites.

Heritage Inventory - Identified in "The Heritage Resources of Maple Ridge, 1998" as having heritage value but not yet officially recognized. Statements of Significance have not been prepared for these sites.

Heritage Potential - There are a number of sites within the area boundaries that have high potential for inclusion on the Maple Ridge Heritage Inventory or Register. These sites have not been fully researched or evaluated, but they make a strong contribution to neighbourhood character and have been flagged for their potential heritage value.

Heritage Character - These sites contribute to the heritage character of the neighbourhood. They are generally modest in scale, pre-1940s wood frame structures that reflect the working-class nature of the area. Some of the sites have been significantly altered, but may have potential for sensitive renovation. These sites have been identified at this time through a visual survey only, and are noted as supporting overall neighbourhood character.

Lots identified in the heritage character area and categories within other areas are key to the unique character, look and feel of the neighbourhood overall. As such, their importance is significant to the neighbourhood's future identity. Redevelopment of sites within the neighbourhood, both those which are situated directly in this historic area as well as those in the surrounding areas should take great care to support, build on and foster the heritage and historic elements and qualities identified.



Hammond Precincts

Hammond has grown significantly since it was first registered as a Township in 1883 and each phase of new development reflects the time period in which it was built. As a result, specific areas or precincts within the neighbourhood have been identified with the primary goal to maintain and build on the character, look and feel of each one. Specific precinct features may include the era of development, characteristics of the built form including road patterns, historic and/or current land use, a prevailing theme and natural features or elements that support the precinct character.

New development should consider the current form and structure of each precinct and review the intent for development outlined in this section.



Part 2

Neighbourhood Precincts

HAMMOND PRECINCTS

Overview

The Hammond Neighbourhood is structured around the small commercial node on Maple Crescent at the Fraser River and Mill site. The commercial area contains a number of modest early commercial buildings, some dating back to the very early 1900s. This has always been the location of commercial activities, across from the Mill site and adjacent to the CP Rail right-of-way and Fraser River. Over time, additional residential areas developed north of the commercial area and also to the west (see map of Hammond Area Plan Precincts on the following page).

Hammond's character in each of these areas reflects these eras of the neighbourhood's history and development. Upper Hammond, with its tight grid street pattern, transitions north of Hammond Stadium to a more curvilinear pattern. Lower Hammond, west of the CP rail line, remains a grid but with larger blocks and wider streets. The streets of Upper and Lower Hammond streets are set at different angles, with Upper Hammond oriented north-south and Lower Hammond approximately 45 degrees relative to True North. This contributes to a distinct identity within each area, which each have self-contained views.

The Hammond Area Plan Precincts map (following page) outlines the four distinct character area precincts within the Hammond Neighbourhood. The character and defining heritage elements of each precinct are described below. What sets these areas apart is the era of development, road and lot patterns, train tracks, and predominant land use.



Figure 2. Images of Hammond's variety of existing uses, character, look and feel.

Hammond Precincts

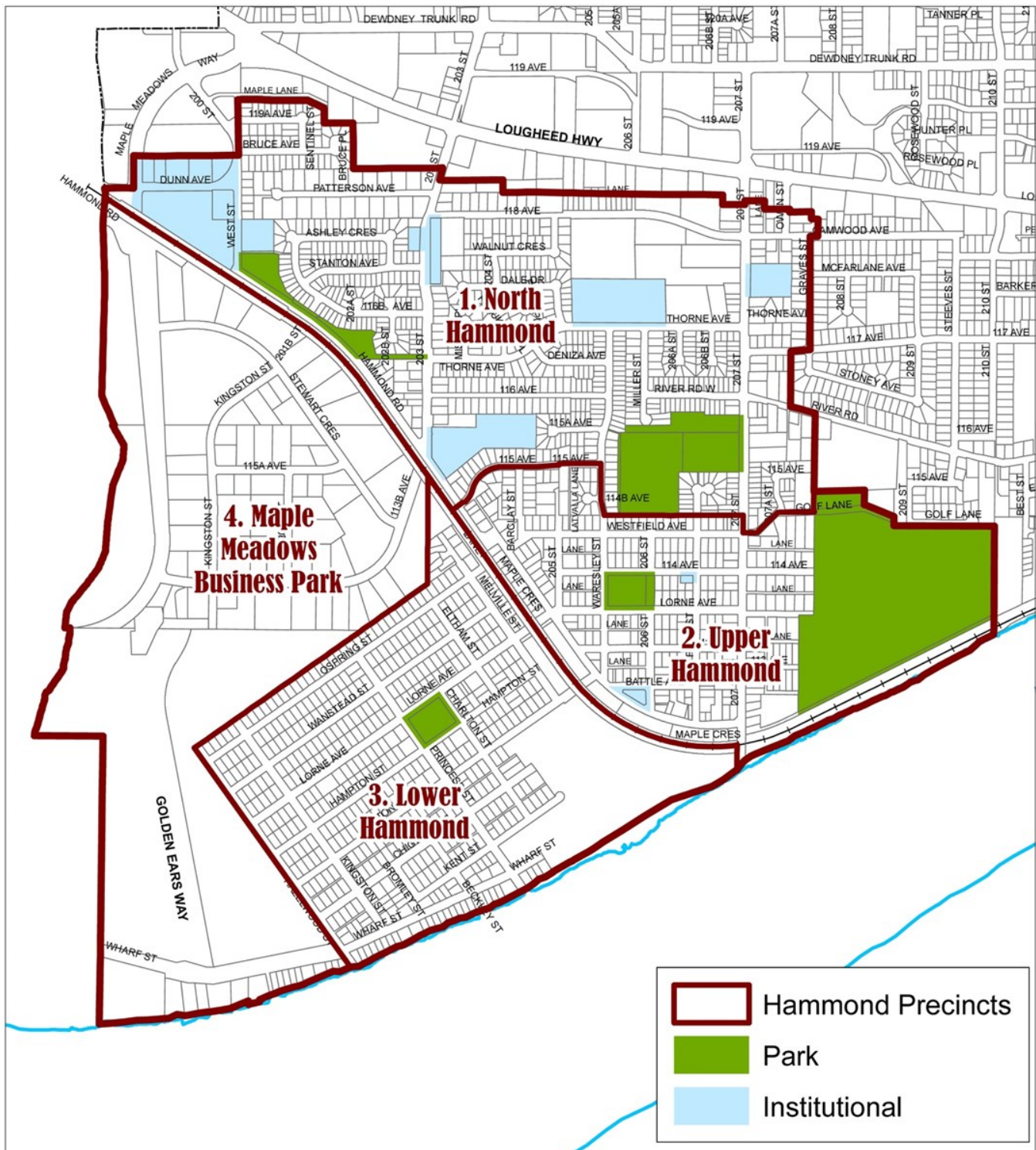


Figure 3. Hammond neighbourhood precincts.

Precinct Description & Key Guideline Concepts for Development

Precinct 1: North Hammond

North Hammond Precinct is located south of Lougheed Highway, east of the CP Rail line, west of Graves Street and north of 115 Avenue and Hammond Stadium. This area of the neighbourhood was developed primarily in the 1980s. The area contains primarily single family homes with some multi-family along 207 Street south of Lougheed Highway and areas along major corridors. This precinct also contains Hammond Elementary School, Ridge Meadows College, Hammond Stadium, and a West Coast Express station located in the Precinct's northwest corner.

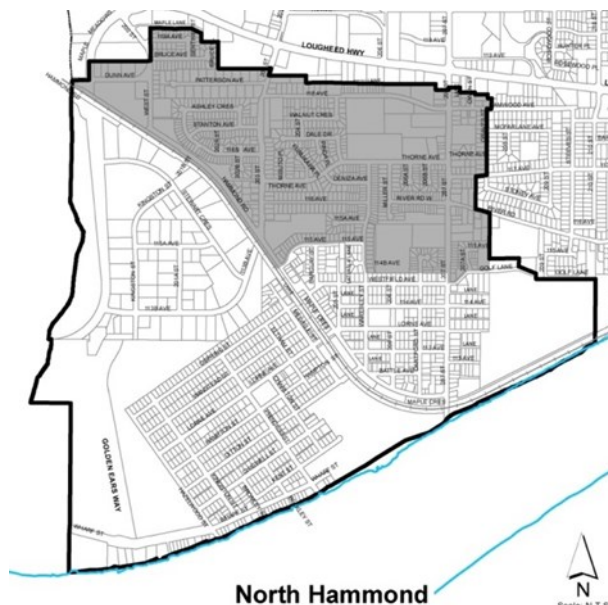


Figure 4. North Hammond precinct (grey).

Key Guideline Concepts

As outlined in the Hammond Neighbourhood Area Plan, the intent of this precinct is to remain largely residential with some redevelopment along major corridors at key nodes and focused sensitive residential infill. With redevelopment, opportunities to increase the connectivity and pedestrian route options in the neighbourhood will be important to better facilitate a walkable development pattern. Redevelopment will also provide an opportunity to incorporate a more refined style.

New development within this precinct should be sensitive to existing character, yet incorporate new development and meets the intent as described above. The following are key concepts to focus on within this precinct. Development Permit Guidelines that support these concepts are most important.

1. Street and Block Pattern

- Increase residential density with a housing style complimentary to existing; and
- Increase pedestrian connectivity of the neighbourhood by connecting to local destinations and public sidewalks.

2. Typical Lot Size and Layout

- maintain the current lot pattern and coverage, look and feel with larger duplex, triplex homes and four-plexes (on corner lots). Use similar massing to other homes in the area, mimicking the same block pattern;
- increase ground-oriented and street fronting townhomes; and
- Siting to take best advantage of sunlight and/or shading in order to plan for future use of solar technology.

3. Greenspace and Landscaping

- increase the number of trees planted on lots and within new development; and,
- ensure high quality & climate change resilient landscaping and common greenspace areas.

4. Housing & Heritage Features

- enhance the neighbourhood look and feel with front façade orientation of new development facing towards the street and parking and driveways that are incorporated in a subtle manner;
- enhance the character and identity of the area with architectural details, themes and materials that speak to Hammond's history and area and/or increase the social and community feel of the neighbourhood. This may include:
 - the use of wood, horizontal wood siding, wood shingles;
 - entrances with porches that overlook the street; and,
 - symmetrical front elevation and second storey with articulated units.



Figure 5. Images of typical development in North Hammond.

Precinct 2: Upper Hammond

Upper Hammond Precinct is the heart of the Hammond Neighbourhood. It is the historic Port Hammond area and contains the small commercial district surrounded by residential uses, Hammond Park and Maple Ridge Golf Course. Upper Hammond is directly adjacent to the CP Rail line and Mill site. This area is important for the commercial core and strong heritage value and character. The smaller blocks and tight grid network of streets create a more traditional, pedestrian oriented, small neighbourhood character, look and feel.

Key Guideline Concepts

Upper Hammond includes the active commercial core area and residential area east of 207th St and the Hammond Golf Course. As outlined in the Hammond Neighbourhood Area Plan, development intent of this precinct is to:

- accommodate new development and density;
- build a strong, viable and vibrant commercial area for Hammond neighbourhood residents, and;
- retain the heritage character and history of this area.

New development within this precinct will be aligned with the existing character of the precinct.

1. Street and Block Pattern

- maintain the tight grid network and walkable nature of precinct
- where larger parcels exist new development should strive to incorporate internal walkways and visual corridors through the development.

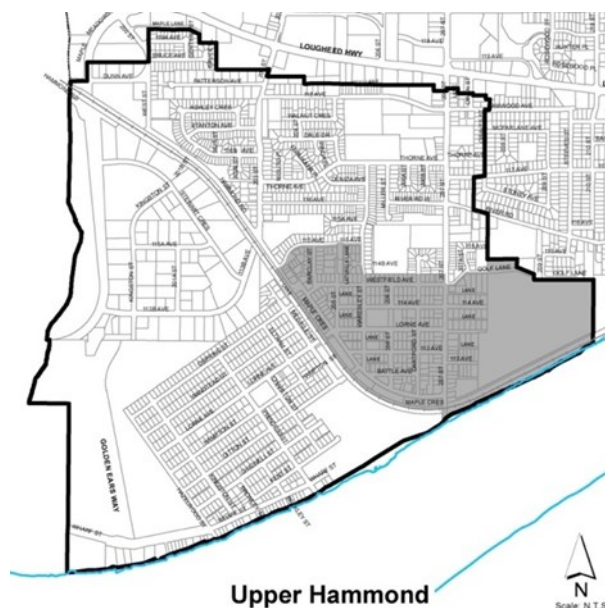


Figure 6. Upper Hammond precinct (grey).



Figure 7. Images of typical Upper Hammond precinct homes

2. Typical Lot Size and Layout

- maintain a sense of scale with development and ensure the massing and scale are broken up to appear consistent with the scale and character of the area .

3. Greenspace and Landscaping

- maintain a high quality and climate change resilient landscape;
- where possible, maintain existing mature trees and shrubs on the lot.

4. Housing & Heritage Features

The following key features are the hallmarks of the heritage style within the Hammond neighbourhood:

- use of wood (for example: wood shingles, horizontal wood siding);
- central front entrance;
- full open front veranda;
- tapered columns;
- shed dormers;
- double hung windows and multi-pane windows, and;
- Symmetrical front elevation and second storey.

New development should be aligned with the development guidelines in the following section and is strongly encouraged to exemplify the character and preserve heritage value where possible. Historic buildings should be retained or facades incorporated into new development. Recycling or reuse of heritage structures and materials is strongly supported.



Figure 8. Landscaping in Upper Hammond



Figure 9. Townhouse development (background) is integrated with an older historic home, taking on a similar style, colours and look.

Other Historic Activities and Community-led Amenities

Celebration of intangible cultural heritage could be continued through further interpretive information that tells the history of the area and stories of people and activities of the past.

- Integrate, recognize and commemorate heritage where identified; and,
- Incorporate heritage in design and architecture of buildings.
- Explore further opportunities for Public Art

Precinct 3: Lower Hammond

Lower Hammond Precinct lies west of the CP rail line, north of the Fraser River and south of the Maple Meadows Business Park. This area includes the Mill site, but is otherwise residential and has developed and redeveloped over time, resulting in a mix of older and newer homes throughout the Precinct.

The Lower Hammond Precinct also has a gridded street network but with larger block sizes, lots and street widths. Bounded by the Maple Meadows Business Park, Fraser River and CP Rail line, this precinct is isolated from the rest of the Hammond Neighbourhood. The precinct area, within the Fraser River Floodplain, is low, flat and fairly homogeneous in land use, with few access and pedestrian connections in and out of the area.

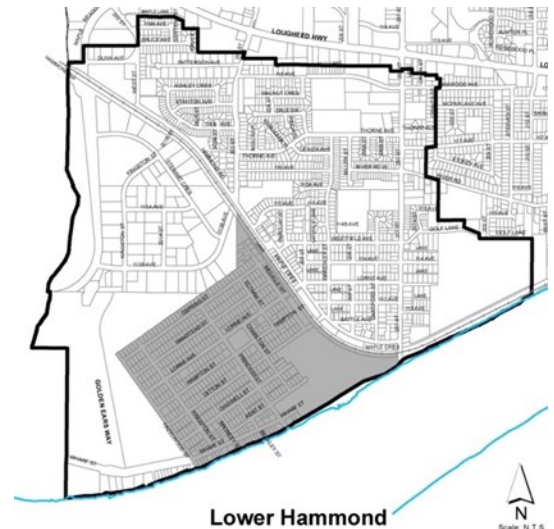


Figure 10. Lower Hammond precinct (grey)

Key Guideline Concepts

1. Street and Block Pattern

- maintain the grid pattern with street facing ground-oriented multi-family housing;
- provide mid-block pedestrian connections through blocks to minimize the length/size of the block.

2. Typical Lot Size and Layout

- provide a variation in land use, and residential density, there enhancing the area's vibrancy with more people and activity in the Precinct .

3. Greenspace and Landscaping

- provide high quality and climate change resilient landscaping,
- where possible, maintain existing mature trees,
- preserve the central park area and make direct pedestrian connections to it.



Figure 11. Images of Lower Hammond precinct homes.

4. Housing & Heritage Features

- Where heritage character homes have been identified, preserve the building, or with new development mimic or draw from the design aspects to reflect in new development.



Precinct 4: Maple Meadows Business Park

The Maple Meadows Business Park Precinct is a key employment node within the Hammond Neighbourhood Area Plan boundary. The area is not well connected to the rest of the neighbourhood overall. However, this area offers nearby business and employment opportunities for the neighbourhood.

The Maple Meadows Business Park uses can be described as discrete and internalized campus-type development. These uses are clustered and not well connected to the adjoining neighbourhood area, in part, due to the nature of the uses contained within.

Key Guideline Concepts

With new development, an opportunity exists to provide, where appropriate, a greater level of connectivity to and through these areas for residents within the adjacent neighbourhood who may be employed here or require more direct non-vehicle access to areas within or through the area to other destinations.

The intent of this precinct is to further develop similar uses to the south, directly adjacent to Lower Hammond and therefore the interface between this area and residential will be an important consideration.

Here, the intent is to:

- Enhance safe pedestrian and cycling connections at the site and neighbourhood level (including connections the trail networks in Pitt Meadows)
- Provide climate appropriate and green features
- Reflect Hammond's history in a contemporary way
- Provide buffer between non-compatible uses

Street and Block Pattern

- New development in this area should seek to provide safe pedestrian connections where opportunities exist.

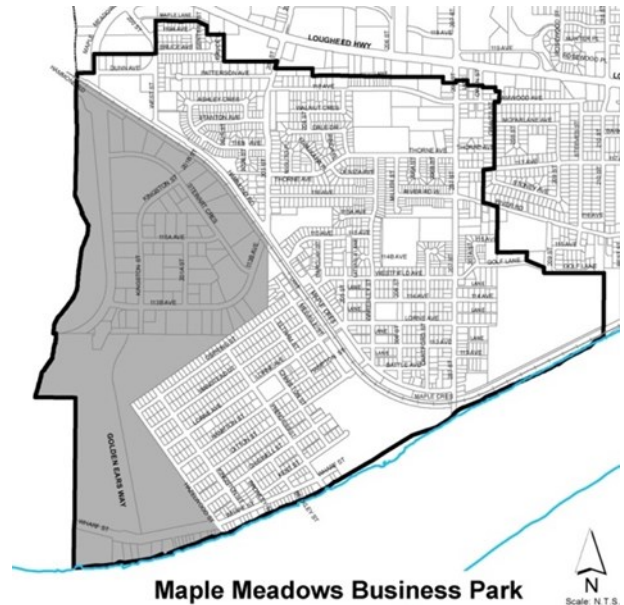


Figure 12. Maple Meadows Business Park precinct (grey).



Figure 13. Images of the Maple Meadows Business Park.

Greenspace and Landscaping

- The intent of new development is to ensure a high quality and climate change resilient landscaping (including tree lined paths or sidewalks, landscaped rest areas, adequate lighting, visually pleasing shrubs and groundcover), along pedestrian and bicycling connections through this area.
- Provide well maintained planted vegetated buffers that include trees, shrubs and groundcover plants between non-compatible uses.
- Incorporate public art or decorative, or colourful materials and signage through the area that enhances wayfinding and is pedestrian friendly.



Figure 14. Landscaping in Maple Meadows Business Park



Application of Development Permit Area Guidelines

The Development Permit Area (DPA) Guidelines are based on an examination of the existing conditions and character within each of the Hammond area precincts as described in Part 2, with the intent to either build on, improve, or manage and maintain the character and form of new development within each precinct while allowing for some change and sensitive residential infill distributed over the entire neighbourhood area. The Hammond DPA Guidelines should be read with the Hammond Area Plan.



Part 3

Development Permit Area Guidelines

Application

Development Permit Area (DPA) Guidelines for form and character may be established for multi-family, commercial, mixed-use and industrial forms of development. DPA Guidelines help inform building and site design so that new development compliments and reinforces the existing character. This includes information on the use of appropriate building materials, siting, colours, design features, landscaping, green space and energy efficiency. The *Local Government Act* does not allow for Development Permit Guidelines to regulate form and character for single family.

Key Guideline Concepts are established in Part 2 for the purpose of identifying focus areas within each precinct and for assessing development permit application for sites in the Hammond Area Neighbourhood and should be reviewed with these guidelines prior to development application.

As designated in the Hammond Area Neighbourhood Plan, the following Development Permit Guidelines apply to all new development of:

- Multi-Family Residential
 - Low Density Multi-Family
 - Medium Density Multi-Family
- Hammond Village Commercial
- Infill General Employment

Concept sketches, diagrams and images are provided with the guidelines to illustrate how the objectives of the development guidelines might be achieved through design.

1.0 General Guidelines

The following general guidelines are specific to energy efficiency and water conservation. These apply to all land use designations and new development. Additional sustainability related guidelines (for example, guidelines related to climate change resilient landscaping, support for walkable communities, design for health, accessibility, etc.) are incorporated throughout each land use designation as suitable.

1.1 Energy Efficiency and Water Conservation

- 1.1.1 Where the street grid allows, orient buildings towards the south, with the long axis running east-west. A southern building orientation is ideally achieved on south-facing lots with minimal obstructions that can block solar access (sun/shade analysis can identify the impact of obstructions).

- 1.1.2 Locate larger windows on the south-facing facade whenever possible for maximum winter solar gain and natural light. Limit the size of windows on the north facade to limit heat loss. With this, provide deciduous trees in front of south facing windows or shades on south facing windows to provide summer shade.



Figure 15. Maximize opportunities for solar gain and solar collection.

- 1.1.3 Use deciduous trees on the southern and western-facing side of a building to maximize the warming effect of solar radiation in winter months and the cooling effect of shade in summer months.
- 1.1.4 Where needed, use window overhangs and/or fixed operable shading devices to control solar gain.
- 1.1.5 Where compatible with existing development within each precinct, choose roof shape and orientation to maximize passive solar gain and opportunities for solar energy collection.
- 1.1.6 Vary height, rooflines and massing to reduce shade on neighbouring buildings and optimize sun exposure for heat gain and daylight.
- 1.1.7 Allow for collection of water from roof downspouts and/or direct drainage to planted areas or into rain barrels for irrigating non-edible plants and landscaping, per best management practices for stormwater management.

2.0 Multi-family Residential (Low and Medium Density)

Low density multi-family includes townhouse, fourplex and courtyard development forms. **Medium density multi-family** includes townhouse, rowhouse and apartment.

2.1 Siting

- 2.1.1 All new development should be oriented toward the street.
- 2.1.2 Buildings on corner sites should be treated as if they have two main façades.
- 2.1.3 Where possible, new development should be sensitive to existing building setbacks, and comply with the siting restrictions of the Zoning Bylaw.
- 2.1.4 Rear yard setbacks may vary from the established pattern, in accordance with existing development regulations, to accommodate development on irregularly shaped lots.

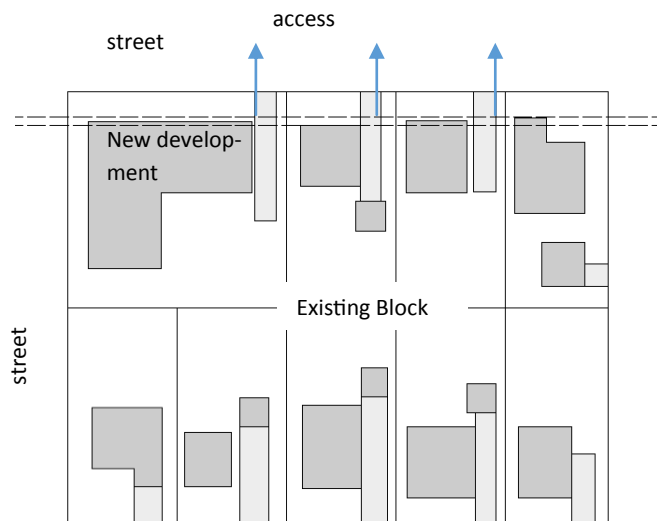


Figure 16: Maintain the typical pattern of access along the block and building on corner sites should front both streets..



Figure 17: Siting Development Permit Guidelines

- 2.1.5 New development shall provide access to parking that maintains the established pattern of lot access within the existing block (see section 1.3 parking and access for additional guidelines).

2.2 Massing

- 2.2.1 New development should mimic existing development and attempt to match the established massing and pattern of the existing streetscape within each precinct.
- 2.2.2 New development with large buildings or groups of units should be articulated to break up the size and massing of the development.
- 2.2.3 Height and roofline of new development or renovated buildings should be consistent in slope and style with the typical pattern established within the precinct. Heights must also comply with the Zoning Bylaw.
- 2.2.4 Apartment buildings over 2 storeys should articulate or step back upper storeys of buildings (the third storey and above) to reduce the scale and massing of the building.
- 2.2.5 New development should provide a transition in scale to adjacent land uses with a different land use designation. This can be achieved through:
- Building design articulation of building features;
 - Setback or buffer to adjacent development, and;
 - A combination of the above with landscaping and trees.



Figure 18: Example of a duplex that reduces the prominence of front vehicle access and garage by emphasizing the entrance and verandah.



Figure 19: Image showing a tri-plex with heritage character and front access with parking at the side.



Figure 20. Articulated building face breaks up the massing.



Figure 21: Guideline 1.1.9 A transition of scale and density for adjacent land uses.

2.3 Parking and Access

- 2.3.1 Rear lane access is preferred, where feasible.
- 2.3.2 Where front access and garages are required, one or more of the following strategies should be applied:
- Smaller shared parking areas or driveways to the side or rear of the units or building;
 - Where a front loaded unit is necessary, the garage should be set back from the primary entrance;
 - Ensure other building elements (such as porches, trellises, landscaping, etc.) act as key focus points to the street face, or;
 - Locate the garage partially below grade (except in floodplain area);
 - Use of garage doors with larger windows or other decorative elements.
 - Use shared parking driveways to reduce the number of crossings at the sidewalk.
- 2.3.3 A covered and secure area for bike lock up and storage should be provided and located with easy and direct access to bike routes, trails and/or public right of ways.
- 2.3.4 Where possible, pedestrian and cycling connections should be improved or created with new development, linking into the local and broader community.
- 2.3.5 The creation of new laneways should be considered, where appropriate and feasible with new development and used as secondary vehicular and pedestrian access .



Figure 22. 4 storey apartment building steps down to three to transition to adjacent existing two storey single family.



Figure 23. Childrens play space within a multi-family development.



Figure 24: Landscaping in parking areas.

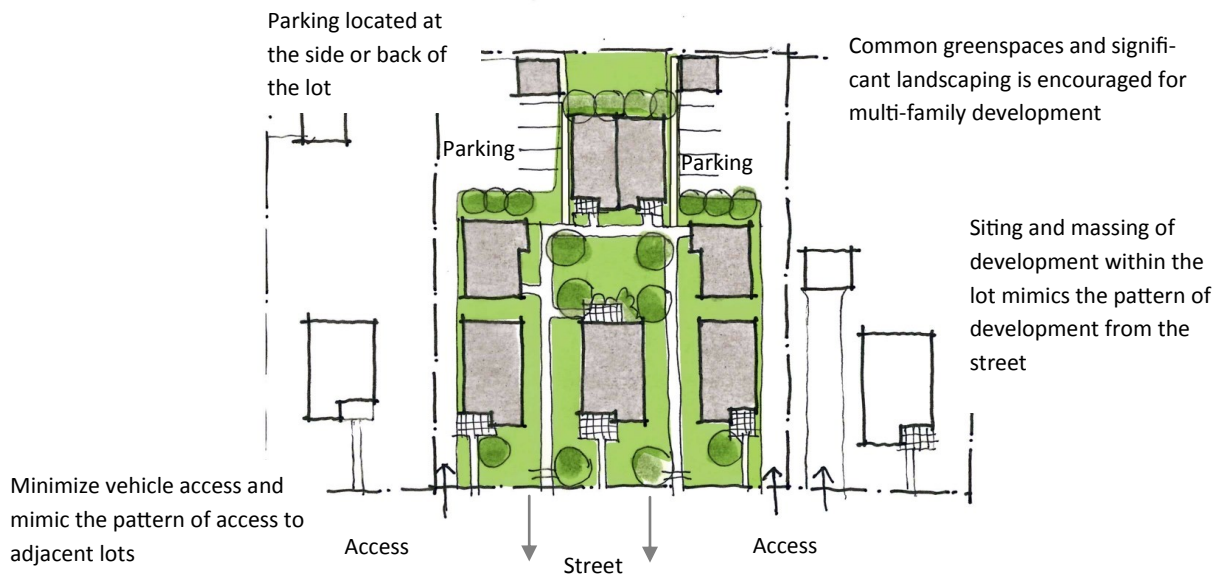


Figure 25: Siting Development Permit Guidelines

2.4 Landscaping and Private Outdoor Spaces

- 2.4.1 Retain existing greenspace, natural assets and landscaping typical to the precinct. Where possible, maintain and incorporate into the site plan, significant tree stands or single mature trees already existing on a building site, in accordance with the Tree Protection and Management Bylaw.
- 2.4.2 Use a vegetation buffer to conceal from view all utility areas, parking areas, and along pedestrian walkways to provide screening, while maintaining visibility for security purposes.
- 2.4.3 Incorporate landscaping within driveways or parking areas, such as planters, trees, landscape strips, or permeable paving.
- 2.4.4 All new multi-family development should create private and semi-private common green space and social areas, for example, a courtyard, covered seating areas, children's play space, common garden plots, and small private yard spaces.



Figure 26: Provide direct pedestrian pathways within new developments.



Figure 27: Landscape buffers provide screening to parking and utility areas, soften and mark walkways.

- 2.4.5 New development shall incorporate pedestrian pathways within the development that directly link to key destinations, such as parking areas, public rights-of-ways, nearby transit stops or amenity destinations.

2.5 Architectural Patterns and Materials

- 2.5.1 Maintain the established use of materials where significant heritage value of the precinct is recognized and has been identified. New development should include more than one of the following:

- use of horizontal wood siding;
- use of wood shingles;
- an open front, and central veranda;
- use of straight or tapered columns;
- shed dormers; and,
- use of multi-paned windows.

- 2.5.2 A symmetrical front elevation for new development is encouraged.

- 2.5.3 The total area of windows/doors (including front porches) should be similar to the area of wall surface.



Figure 28. Semi-private seating/waiting area along the street next to multi-family development.



Figure 29. Multi-family development with a small verandah, overhang, and use of horizontal wood siding.

Use of similar style and rooflines to existing development



Minimize scale differences of new development by stepping down and using landscaping to buffer

Figure 30: New multi-family development shall borrow existing styles and materials.

Utilities, recycling and garbage

- 2.5.4 Screen all external services (meters, connections), storage, loading and utility areas with landscaping or decorative fencing.
- 2.5.5 Provide for recycling, green waste and garbage bin storage to be enclosed in a building or shelter, with a design that borrows from the architectural vocabulary of the main building on the site.
- 2.5.6 Shelter rooftop utilities and infrastructure from view.



Figure 31: Use of wood siding, columns, and multi-paned windows.



Figure 32: Direct internal pedestrian access through a multi-family development connecting to sidewalks.

3.0 Hammond Village Commercial

Hammond Village Commercial lands are primarily located in Hammond's historic commercial node at Maple Crescent and Dartford Street, within the Upper Hammond Precinct.

3.1 Siting

- 3.1.1 The front face of the buildings should abut sidewalk and property line.
- 3.1.2 Where buildings must be set back from the front of the property line:
- Use landscaping to create small outdoor public spaces, such as "pocket parks" and courtyards;
 - Where possible, provide outdoor patio space in relation to the building use;
 - At a minimum, incorporate decorative planters, benches and trees.



Figure 33. Shops set at property line and sidewalk, facing the street.



Figure 34: Mixed use with residential units above and smaller retail shops below.

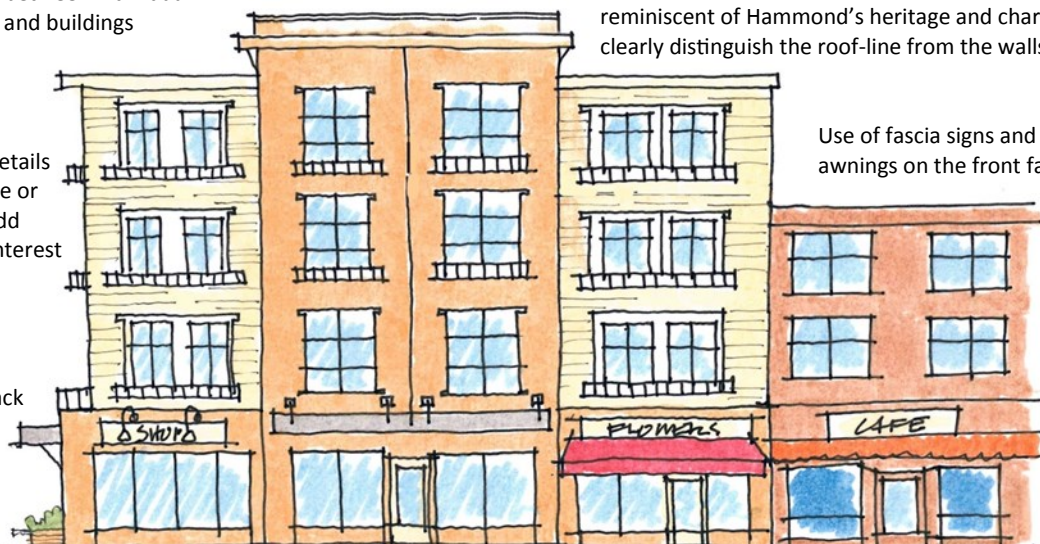
3.2 Massing

- 3.2.1 Incorporate large areas of glazing (windows) to create visual interest and enable views into and out of the businesses. A minimum 75% glazing on the ground floor is desired.

Pilasters and columns to distinguish between individual businesses and buildings

Use of trim details and landscape or planters to add variety and interest

Slight step back to reduce massing



A cornice, overhang or other decorative motif reminiscent of Hammond's heritage and character to clearly distinguish the roof-line from the walls of

Use of fascia signs and 4 point awnings on the front face

Smaller retail units with transparent fronts and clearly marked entrances

No front set back

Figure 35: Mixed use development, articulated horizontal massing, smaller retail units, transparent store fronts.

- 3.2.2 Maintain smaller commercial retail units at ground level, not bigger than 300m² (3,000 sq.ft.).
- 3.2.3 Incorporate frequent entrances along the street. Where appropriate, recess entryways to provide weather protection and further identify the entrance.
- 3.2.4 Incorporate functional weather protection, awning, canopies and overhangs into facades with no front setback that correspond to the placement of doors and windows.
- 3.2.5 Articulate the design of buildings to accentuate building edges, corners and entryways.

3.3 Parking and Access

- 3.3.1 Locate parking at the rear or side of buildings.
- 3.3.2 Where parking to the side is necessary, provide a landscape edge to define the street edge and pedestrian realm. Incorporate seating, and architectural materials and form to reflect the building vernacular.
- 3.3.3 Where possible, include a dedicated parking space to car share vehicles and an electric vehicle charging station.
- 3.3.4 Provide covered, secure bike lock-up areas near main entrances.
- 3.3.5 Provide direct accessible pedestrian sidewalks from parking areas to entrances and from entrances to public streets and sidewalks.



Figure 36: Landscape screening and side parking lot.



Figure 37: Provide for outdoor patio space and allow activity to spill out onto the street.

3.4 Landscaping & Public Realm Amenity

- 3.4.1 Use a vegetation buffer to conceal from view all utility areas, parking areas, and along pedestrian walkways to provide screening, while maintaining visibility for security purposes



Figure 38: Seating areas, paving patterns and planters ensure a high quality and inviting public realm within commercial areas.

- 3.4.2 Incorporate landscaping within driveways or parking areas, use planters at entrances, trees, landscape strips, or permeable paving to incorporate additional greenspace.
- 3.4.3 Where parking is visible from a fronting or flanking street it should be screened with trees, plants or decorative fencing.
- 3.4.4 Where possible, incorporate and integrate public art within plaza areas, courtyards, infrastructure, sidewalks, etc.
- 3.4.5 Incorporate public amenities with new development, for example, seating/resting areas, landscape strips, planters and paving patterns.



Figure 39. Incorporate public art that references Hammond's history, adds vibrancy, entertains or educates.

3.5 Architectural Patterns and Materials

- 3.5.1 Incorporate architectural materials and features that reflect the Hammond area or add to the historic qualities and character of the area. This may include:
 - Incorporating a front parapet;
 - Use of masonry (paving, stones, brick patterns, etc.);
 - Vertical and/or tapered columns;
 - Ornamental or integrated artwork;
 - Integrated architectural lighting; and,
 - Trim details and moldings;
 - Multi-paned windows
- 3.5.2 Incorporate colour and decorative details within building design.



Figure 40. Use multi-paned windows to create a historic look and feel.



Figure 41. Incorporate patterns and materials that reflect aspects of Hammonds heritage.



3.6 Transition of Use and Vacant Lot Improvement

- 3.6.1 Keep vacant lots clean, clear and grassed in accordance with the Regulation of Untidy and Unsightly Premises Bylaw. Temporary or seasonal uses are strongly encouraged (e.g. outdoor patio, market, pop up store, sitting area, garden spaces, etc.) with appropriate permits.
- 3.6.2 Where fencing is required on a vacant lot, use transparent and decorative fences that maintain sight lines between the sidewalk and entrances to adjacent buildings. Steel, aluminum, wood or vinyl are acceptable materials. Chain link fencing is strongly discouraged except at the rear lot line or to separate the lot from industrial uses. Subject to the Zoning Bylaw regulations.



Figure 42. Commercial open to the street, incorporating patio space, landscaping and nearby bike parking.

3.7 Signage

- 3.7.1 Provide signage that is complimentary to building architecture and materials and that clearly identifies uses and shops.
- 3.7.2 Signage shall be directed at pedestrians.
- 3.7.3 Provide visible signage identifying the building address at all entrances.
- 3.7.4 Limit the total number of signs to a maximum of three (for example, entrance sign, awning sign and sandwich board) to reduce visual clutter and make individual signs easier to read.
- 3.7.5 Representational and iconic signage (for example, signs that reference Hammond's History) are encouraged to supplement conventional text-based signs. This will help establish the special character of Hammond's small commercial core.



Figure 43: Recessed, clear entry with weather protection awning and signage above and on the window.



Figure 44: Smaller retail spaces, multiple entries, transparent front and visible signage on the canopy.

3.7.6 A single external sign band may be applied to each façade at the first storey, and should not exceed 1.0m in height along any length.

3.7.7 A minimum clearance of 2.3m should be maintained for signs projecting over the sidewalk or other public space.

3.7.8 The following are preferred and acceptable types of signage in the Hammond Village commercial designation (see image examples on the following page):

- Projecting two dimensional or blade signs suspended from canopies and awning (fitting within a 92cmx153cm (36"x60") horizontal rectangle);
- Externally lit signs;
- Small vertical banners and signs; and
- Temporary sandwich board signs located on the sidewalk, and out of the direct flow of pedestrian traffic.

3.7.9 Any free standing signs should incorporate architectural features and materials used by the main building on the site.



Figure 45. Temporary sandwich board signs placed out of the main flow of pedestrian traffic can add to the street life and look.

3.8 Lighting

3.8.1 Illuminate building facades and features by providing architectural lighting on the face of buildings.

3.8.2 Provide pedestrian scaled lighting with high quality design above sidewalks for night time visibility.

3.8.3 Illuminate paths and entry areas sufficiently to ensure pedestrian comfort and safety.

3.8.4 Soft white LED lighting is preferred in public areas.



Figure 46: Small recessed courtyard shops with blade signage and potted planting at entrance add character and a welcoming feel..

- 3.8.5 Minimize light pollution and ensure lighting is sensitive to nearby residential uses. Avoid visible, glaring light sources by using down lights or up lights with cut-off shields.
- 3.8.6 Gooseneck lights and sconces applied to fascia underneath weather protection elements are the preferred types of storefront lighting.
- 3.8.7 Incorporate valence lighting into canopies and up-lighting to illuminate pathways.
- 3.8.8 Use of LED lighting for storefronts and seasonal lighting on street trees is encouraged.
- 3.8.9 Avoid the use of exterior fluorescent light sources.
- 3.8.10 Incorporate architectural glare free lighting into the canopy soffit that has either a low-level light source or one not directly visible to pedestrians.



Figure 47: Gooseneck lighting highlights signage and directs light.

3.9 Utilities, Recycling and Garbage

- 3.9.1 Screen all storage, loading and utility areas or incorporate landscaping or decorative fencing.
- 3.9.2 Provide areas for recycling collection, composting and waste disposal that are appropriately sized, screened from view using an enclosure that reflects the materials and architecture of the main building on site, are easily accessible and have capacity for future expansion.
- 3.9.3 Shelter rooftop utilities and infrastructure from view.

4.0 Infill General Employment

The Infill General Employment designation is applied to mostly underutilized lands adjacent to the railway tracks in Precincts 2 (Upper Hammond) and 3 (Lower Hammond). These lands are narrow and limited in potential for redevelopment. The intent of Infill General Employment is to create opportunities for businesses that do not generate much traffic and have minimal need for public parking. Additionally, this designation is limited to businesses that do not produce much noise, odor, or fumes and which blend well into the nearby residential areas through attractive building design and landscape screening.

4.1 Siting

- 4.1.1 All new development should address the street with a clear and visible entrance.
- 4.1.2 Where possible, new development should have a reduced or zero front yard setback.
- 4.1.3 Ensure direct pedestrian connections to the nearest public right of way and to adjacent lots.
- 4.1.4 Pedestrian access to the site should be well marked and accessible. Connect front doors and public streets with paved sidewalks.



Figure 48: Development addresses the street and blends well with surrounding residential area.

4.2 Massing

- 4.2.1 Avoid blank walls. Where walls without windows are necessary, decorative architectural details and materials, landscape screening, or artwork is encouraged.
- 4.2.2 Large developments should be articulated, include varying height and rooflines or use decorative materials to break up the mass and scale of the building.
- 4.2.3 Incorporate outdoor patios, plazas or amenity areas with comfortable seating and gathering areas.



Figure 49: Pedestrian and bicycle amenities are strongly encouraged.

4.3 Parking, Access, Loading and Utility Areas

- 4.3.1 Locate parking areas at the side and rear of buildings.
- 4.3.2 Where parking is visible from a fronting or flanking street, the layout should be enhanced with trees, plants and/or decorative fencing.
- 4.3.3 Screen all storage, loading and utility areas or incorporate landscaping or decorative fencing.
- 4.3.4 Provide areas for recycling collection, composting and waste disposal that are appropriately sized, screened from view using an enclosure that reflects the materials and architecture of the main building on site, are easily accessible and have capacity for future expansion.
- 4.3.5 Where possible, include a dedicated parking space to car share vehicles and an electric vehicle charging station.
- 4.3.6 Provide pedestrian amenities and bicycle parking on site in a convenient and covered location.



Figure 50: A friendly face to the street, use of landscape, clear entrances, and pedestrian amenities help employment buildings blend well with nearby residential.

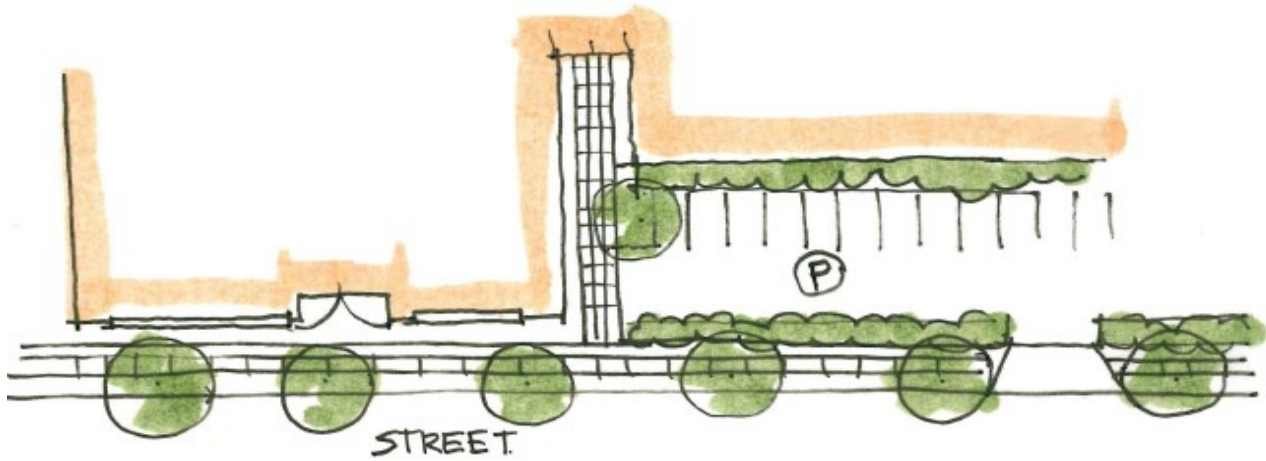


Figure 51: Parking located to the side or rear of the building, where visible from the street should be screened with landscaping.

4.4 Landscaping

- 4.4.1 Maintain mature tree stands and vegetation on site and replace trees that were removed during site development with trees suited to the climate and soil conditions (Maple Ridge Tree Protection and Management Bylaw).

- 4.4.2 New development shall ensure direct and accessible pedestrian connections to public rights-of-way and/or local trail networks and cycling pathways within new or existing green corridors to encourage alternative modes of transportation.
- 4.4.3 All new development should include high quality landscaping at entrances, along pedestrian pathways.
- 4.4.4 Apply climate change resilient, 'xeriscape' or low water use landscaping techniques to minimize and/or eventually eliminate the need for irrigation.
- 4.4.5 Plant shade trees and vegetation in paved open spaces, in particular to break up larger parking areas, line internal roads, sidewalks and driveway access.
- 4.4.6 Fencing of the lot perimeter is not permitted except where safety or storage of materials are of concern. In this case, landscape screening with fencing and only in this designated area within the lot is encouraged to provide a more attractive visual appearance.

4.5 Architectural Patterns and Materials

- 4.5.1 New development should incorporate architectural materials and features that reflect Hammond's history and/or where a more contemporary style is desired, add to the unique character of the area and exhibit a design response to the location and context. Some examples of contextual components to draw on include:

- the railway;
- the Fraser River; or
- the cedar mill/forest industry.

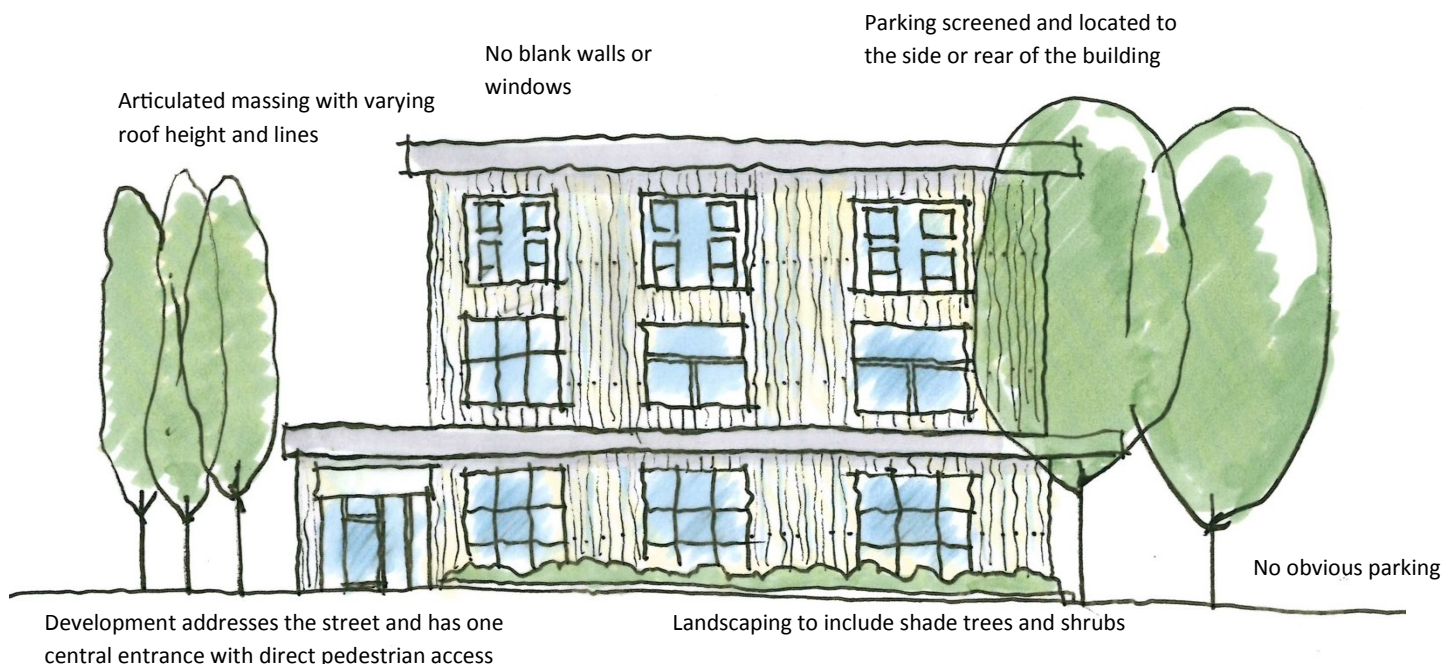


Figure 52: General employment guidelines.

- 4.5.2 In order to blend into the adjacent residential area, new development shall minimize the number of entrances. A common entrance should be used where there are multiple units in one building. Entrances shall be clearly marked and addressed.

4.6 Signage

- 4.6.1 All freestanding signs should be located in a landscaped area and/or incorporated in the design of the building. They should be no higher than the first storey of the primary building on the site they identify. A freestanding sign should incorporate architectural features and materials used by the main building on the site.
- 4.6.2 All other signage providing directional information or identifying the purpose of buildings should be no more than 1 m (3 ft.) wide near or over doors and windows intended for public access.

4.7 Lighting

- 4.7.1 All walkways, driveways, entrances and pedestrian pathways should be adequately lit with energy efficient lighting.
- 4.7.2 Minimize light pollution and ensure lighting is sensitive to nearby residential uses. Avoid visible, glaring light sources by using down lights or up lights with cut-off shields.

4.8 Transition of Use and Vacant Lot Improvement

- 4.8.1 Keep vacant lots clean, clear and grassed.
- 4.8.2 Where fencing is required on a vacant lot, use transparent and decorative fences that maintain sight lines between the sidewalk and main building entrance. Steel, aluminum, wood or vinyl are acceptable materials. Chain link fencing is strongly discouraged except at the rear lot line or to separate the lot from industrial uses. Subject to the general regulations of the Zoning Bylaw.

